

Town of Branford  
Project No. 0170-3513GR  
Pedestrian Connectivity Improvements  
Addendum #2

Date: January 24, 2025

Submission Date: February 6, 2025 (Unchanged)

Prospective bidders, and all those concerned, are hereby informed that the following is made a part of the bid documents, which should be amended as follows:

The following questions were received about the RFP, with the answers following:

1. On the top left of page 17 the diagram looks to indicate that the Town is requiring 2 steel reinforcing pins/dowels/bars every 30', however I cannot find the spec for this. Can you provide a little clarity regarding this?

**At the final daily pour, ½” smooth metal dowels treated with bond breaker shall be inserted into the last slab of the sidewalk. Dowels shall extend a minimum of 9 inches into each slab. A pre-drilled asphalt expansion joint shall be placed over the dowels to prepare for the continuation of the sidewalk.**

**Standard details provided in the plan set are missing notes. Attached to this addendum are the standard details with all notes provided.**

2. Can you confirm that there is not a required or not required Prebid for this project?

**A pre-bid is not required for this project. Refer to Section 1. General Terms and Conditions – bullet K and Section 3 of the Supplemental Instructions to Bidders for additional information.**

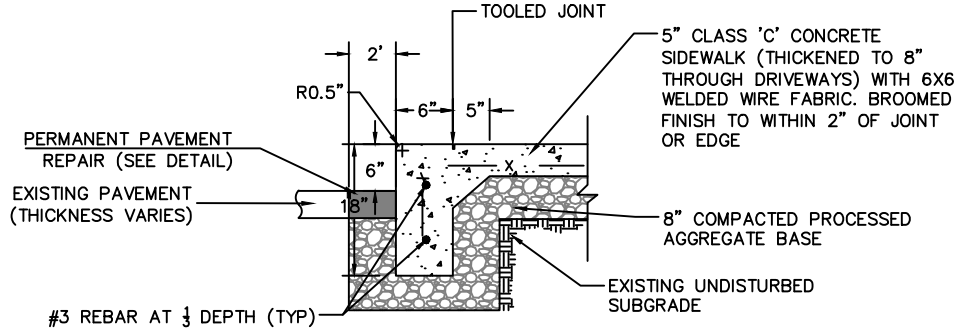
Respectfully Submitted,  
Kevin Ortiz, EIT  
Civil Design Engineer

Please acknowledge receipt of the addendum below and submit with your bid submittal.

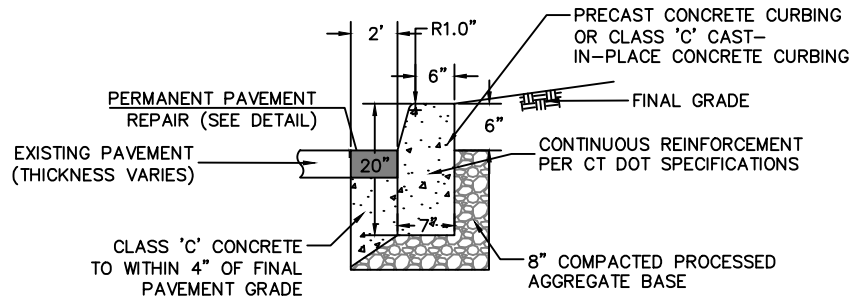
Signature: \_\_\_\_\_ Date: \_\_\_\_\_

Title: \_\_\_\_\_

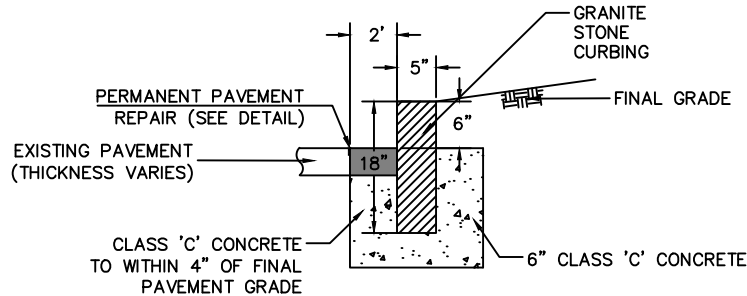
Company: \_\_\_\_\_



### MONOLITHIC CURB AND SIDEWALK



### PRECAST OR CAST-IN-PLACE CONCRETE CURBING

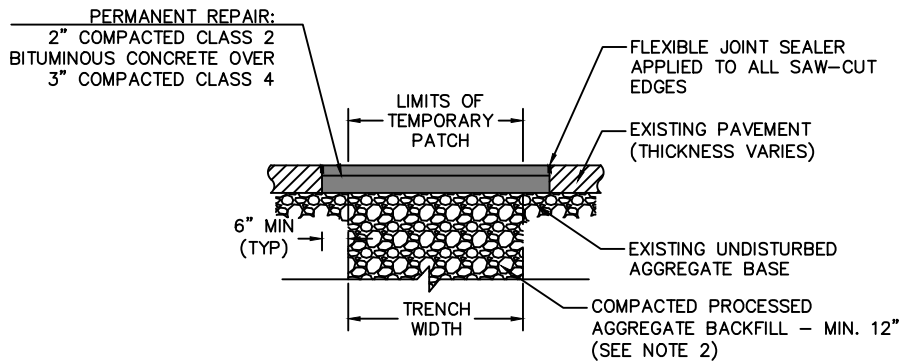


### GRANITE CURBING

**NOTES:**

- 1) ALL STONE OR PRECAST CURBING SHALL CONFORM TO THE STATE OF CONNECTICUT STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES, AND INCIDENTAL CONSTRUCTION, AS PERIODICALLY AMENDED.
- 2) MINIMUM LENGTH OF STONE CURBING IS 4'; MINIMUM LENGTH OF PRECAST CONCRETE CURBING IS 6'
- 3) CURBS OF RADIUS LESS THAN 100' SHALL BE BUILT OF CURVED STONE CURB, CUT OR CAST CONCRETE, AS APPLICABLE.
- 4) ALL STONE CURB SHALL BE BACKFILLED WITH CLASS 'C' CONCRETE TO WITHIN 4" OF FINISHED GRADE.
- 5) ALL OPEN BOTTOM JOINTS BETWEEN STRAIGHT SECTIONS OF STONE OR PRECAST CONCRETE CURB SHALL BE BACKFILLED WITH CLASS 'C' CONCRETE.
- 6) 1/2" PREFORMED EXPANSION JOINT FILLER SHALL BE USED NOT MORE THAN 20' ON CENTER FOR PRECAST OR CAST-IN-PLACE CONCRETE.
- 7) JOINTS BETWEEN STONE CURB SHALL NOT EXCEED 1/4". JOINTS BETWEEN STONE CURB AND CONCRETE CURB SECTIONS SHALL NOT EXCEED 1/2".
- 8) IF EXISTING PAVEMENT MARKINGS ARE DISTURBED, MARKINGS SHALL BE PROVIDED BY THE CONTRACTOR AT NO COST TO THE TOWN OF BRANFORD.


SUBMITTED BY: KAO		DATE: 5.31.2024	
APPROVED BY: JMH		SCALE: N.T.S	
FILE NAME: CURB DETAILS.dwg			
REVISIONS			
REVISIONS NO.	DATE	BY	DESCRIPTION
<b>TOWN OF BRANFORD CONCRETE AND GRANITE CURBING</b>			

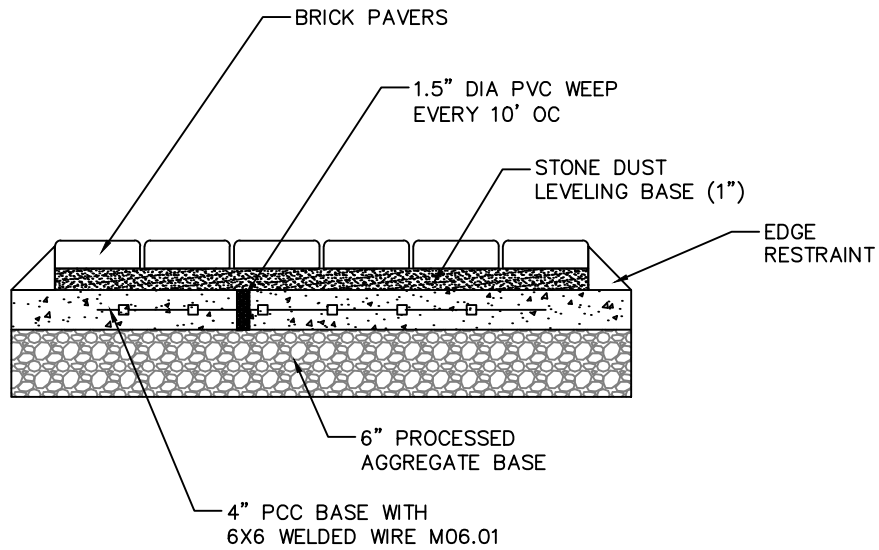


## PERMANENT PAVEMENT REPAIR

**NOTES:**

- 1) TEMPORARY PATCH SHALL BE REMOVED AND SPOIL DISPOSED OF OFF-SITE. LIMITS OF PERMANENT PAVEMENT REPAIR SHALL BE SAW-CUT AT 90 DEGREES TO THE SURFACE.
- 2) UNSUITABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF OFF-SITE AND REPLACED WITH PROCESSED AGGREGATE COMPACTED TO MINIMUM 12" DEPTH.
- 3) PERMANENT PAVEMENT REPAIR SHALL MEET THICKNESS SPECIFIED ABOVE OR MATCH EXISTING PAVEMENT THICKNESS, WHICHEVER IS GREATER.
- 4) PROCESSED AGGREGATE SHALL BE FURNISHED, INSTALLED, AND COMPACTED TO 95% STANDARD PROCTOR DENSITY WHERE TRENCH SETTLEMENT HAS OCCURRED AT NO ADDITIONAL COST TO THE TOWN.
- 5) ALL VERTICAL SURFACES SHALL BE COATED WITH TACK COAT PRIOR TO PAVING.
- 6) FINE AGGREGATES SHALL BE UTILIZED TO PREVENT PICKUP OF FLEXIBLE JOINT SEALER PRIOR TO OPENING TO TRAFFIC.
- 7) IF SUPERPAVE MIX IS TO BE UTILIZED:  
 2" HMA S0.375 PG64-22 SURFACE COURSE OVER  
 3" HMA S1.0 PG64-22 BINDER COURSE
- 8) IF EXISTING PAVEMENT MARKINGS ARE DISTURBED, REPLACEMENT MARKINGS SHALL BE PROVIDED AND PLACED BY THE CONTRACTOR AT NO COST TO THE TOWN OF BRANFORD.

SUBMITTED BY: KAO		DATE: 5.31.2024		
APPROVED BY: JMH		SCALE: N.T.S		
FILE NAME: PERM PATCH.dwg				
REVISIONS				
REVISIONS NO.	DATE	BY	DESCRIPTION	
<b>TOWN OF BRANFORD</b> <b>PERMANENT PAVEMENT REPAIR</b>				



NOTES:

- 1) SAND BEDDING SHALL BE COMPACTED PRIOR TO THE PLACEMENT OF BRICKS/PAVERS
- 2) ALL JOINTS SHALL BE HAND TIGHT AND NOT TO EXCEED  $\frac{1}{8}$ ". POLYMERIC SAND SHALL BE SWEEP INTO ALL JOINTS
- 3) PORTLAND CEMENT CONCRETE SHALL BE 4000 PSI AND REINFORCED WITH 6X6 WELDED WIRE MESH
- 4) FILTER FABRIC SHALL BE GLUED TO EACH END OF WEEP PIPE PRIOR TO PLACEMENT. CARE MUST BE TAKEN DURING PLACEMENT OF CONCRETE TO PREVENT CLOGGING
- 5) EDGE RESTRAINT SHALL BE PROVIDED ON ALL SIDES OF THE BRICK/PAVER SIDEWALK. RESTRAINT SHALL BE CURBING, SIDEWALK, STEEL OR ALUMINUM ANGLE

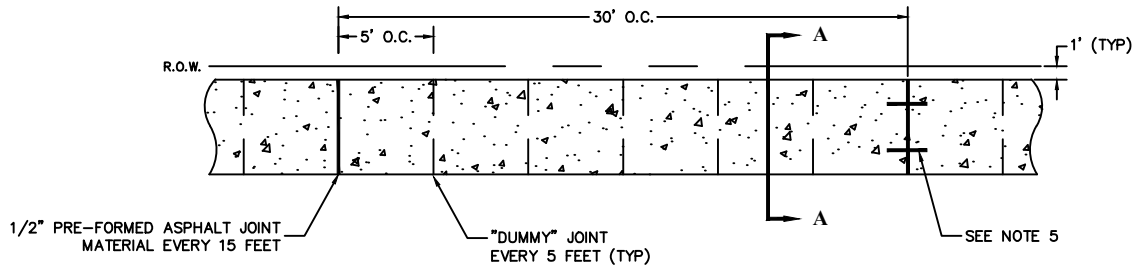
SUBMITTED BY: KAO  
 APPROVED BY: JMH  
 FILE NAME:  
 PAVER WALK.dwg

DATE: 5.31.2024  
 SCALE: N.T.S

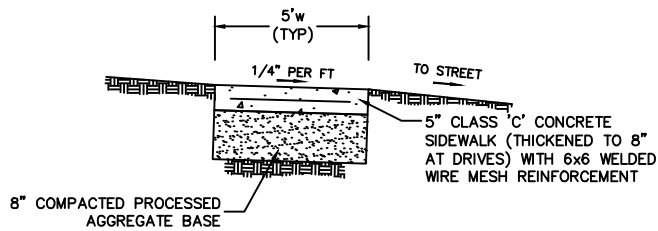


REVISIONS			
REVISIONS NO.	DATE	BY	DESCRIPTION

TOWN OF BRANFORD  
 BRICK PAVER SIDEWALK



SECTION A-A  
TYPICAL SECTION

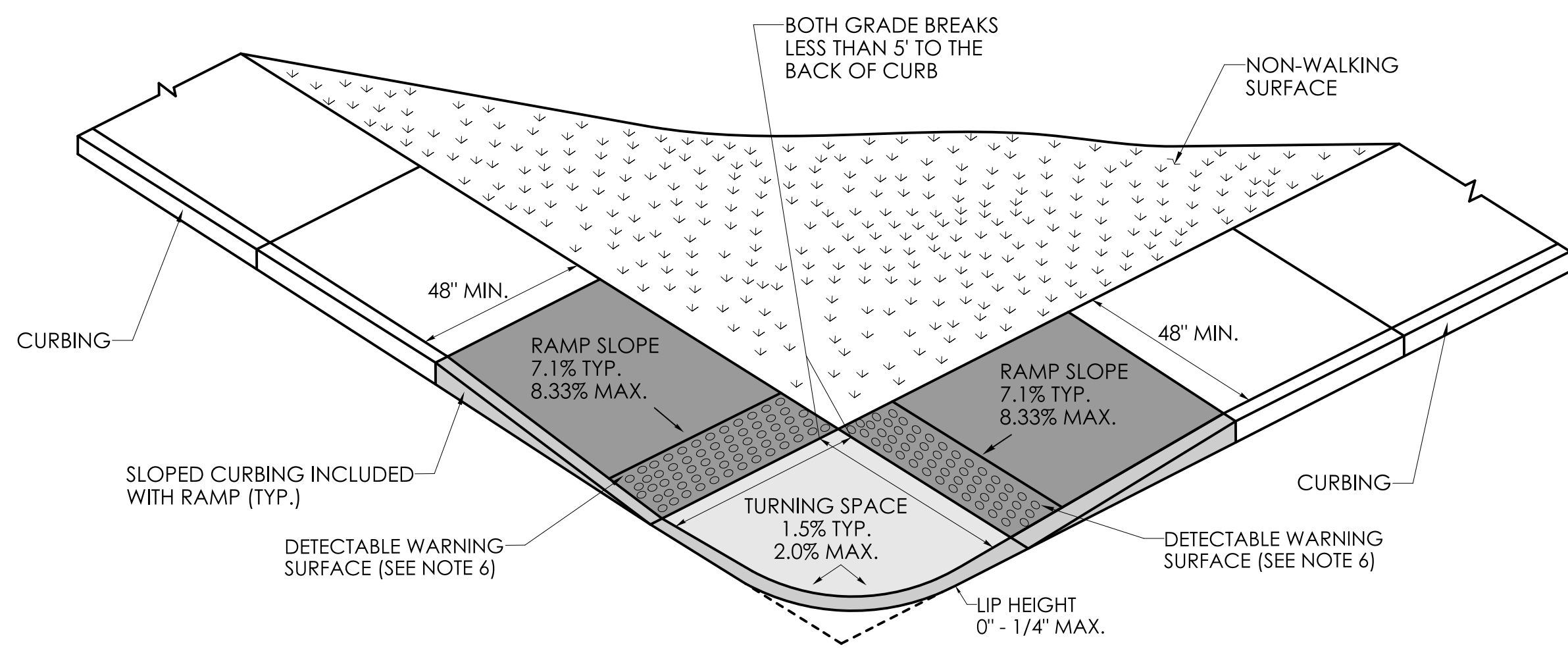


TYPICAL PCC SIDEWALK

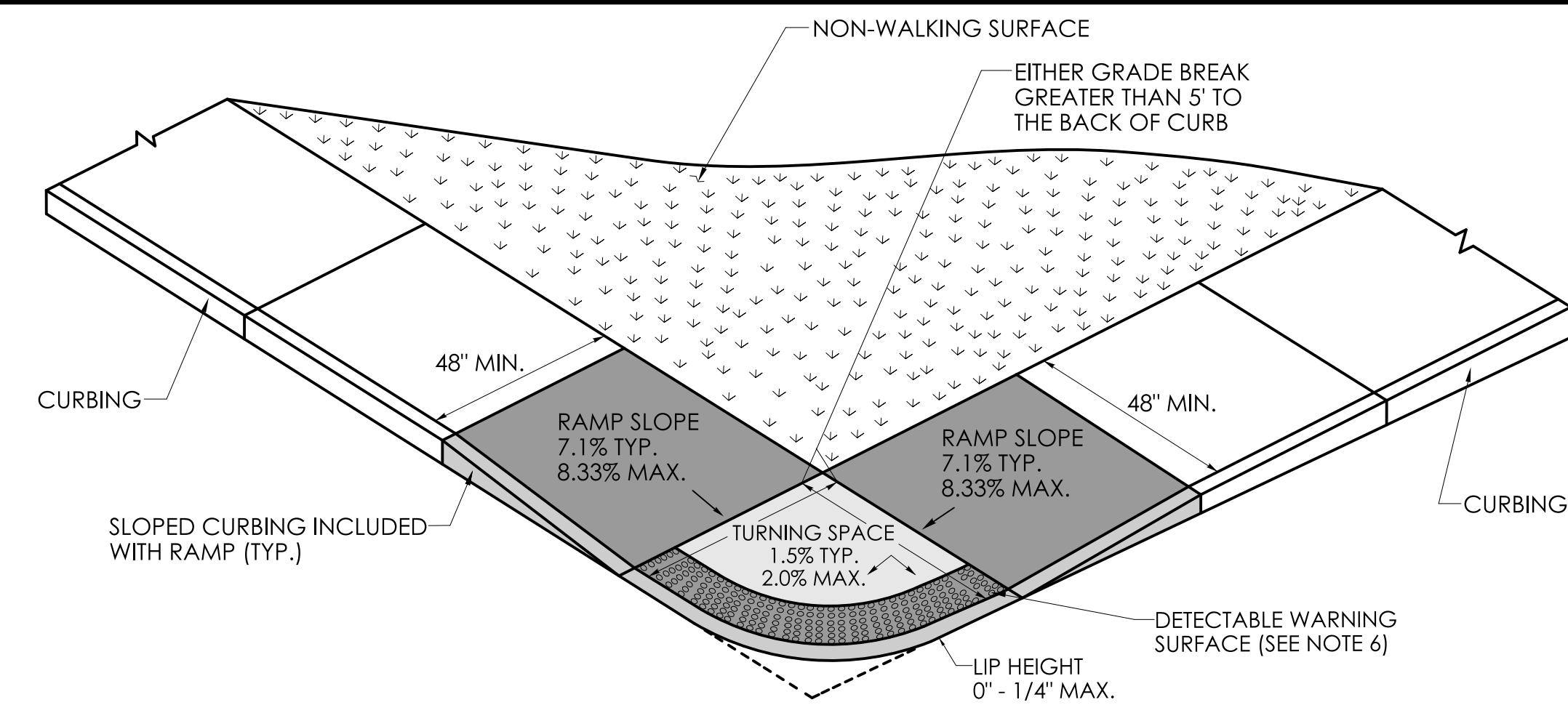
NOTES:

- 1) SIDEWALK CONSTRUCTION SHALL CONFORM TO ALL APPLICABLE REQUIREMENTS OF THE STATE OF CONNECTICUT STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES, AND INCIDENTAL CONSTRUCTION, AS PERIODICALLY AMENDED.
- 2) CONCRETE SHALL BE CLASS 'C' MINIMUM 4,000 PSI.
- 3) SIDEWALKS SHALL HAVE A LIGHT BROOM FINISH TRANSVERSE TO THE DIRECTION OF TRAVEL.
- 4) SIDEWALK WIDTH SHALL BE MINIMUM 5 FEET OR MATCH EXISTING, WHICHEVER IS GREATER. WIDTHS LESSER THAN 5 FEET SHALL BE APPROVED BY THE TOWN ENGINEER OR DIRECTOR OF PUBLIC WORKS.
- 5) AT THE FINAL DAILY POUR, 1/2" SMOOTH METAL DOWELS TREATED WITH BOND BREAKER SHALL BE INSERTED INTO THE LAST SLAB OF THE SIDEWALK. DOWELS SHALL EXTEND A MINIMUM 9 INCHES INTO EACH SLAB. A PRE-DRILLED ASPHALT EXPANSION JOINT SHALL BE PLACED OVER THE DOWELS TO PREPARE FOR THE CONTINUATION OF THE SIDEWALK.
- 6) WHEN POURED AGAINST EXISTING CURB, EXPANSION JOINT SHALL BE PLACED ALONG THE ENTIRE LENGTH OF THE INTERFACE WITH THE SIDEWALK.

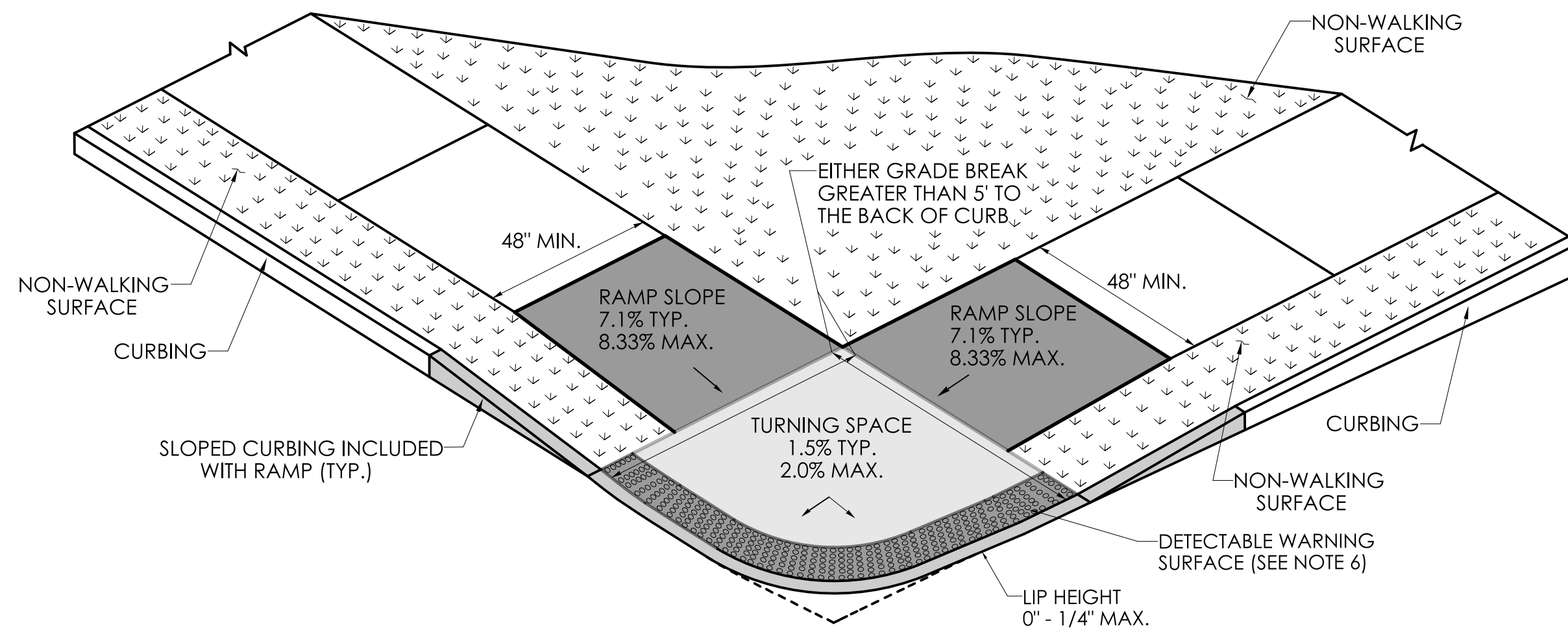
SUBMITTED BY: KAO		DATE: 5.31.2024		
APPROVED BY: JMH		SCALE: N.T.S		
FILE NAME: TYP SIDEWALK.dwg				
REVISIONS				
REVISIONS NO.	DATE	BY	DESCRIPTION	
<p><b>TOWN OF BRANFORD</b> <b>CONCRETE SIDEWALK</b></p>				



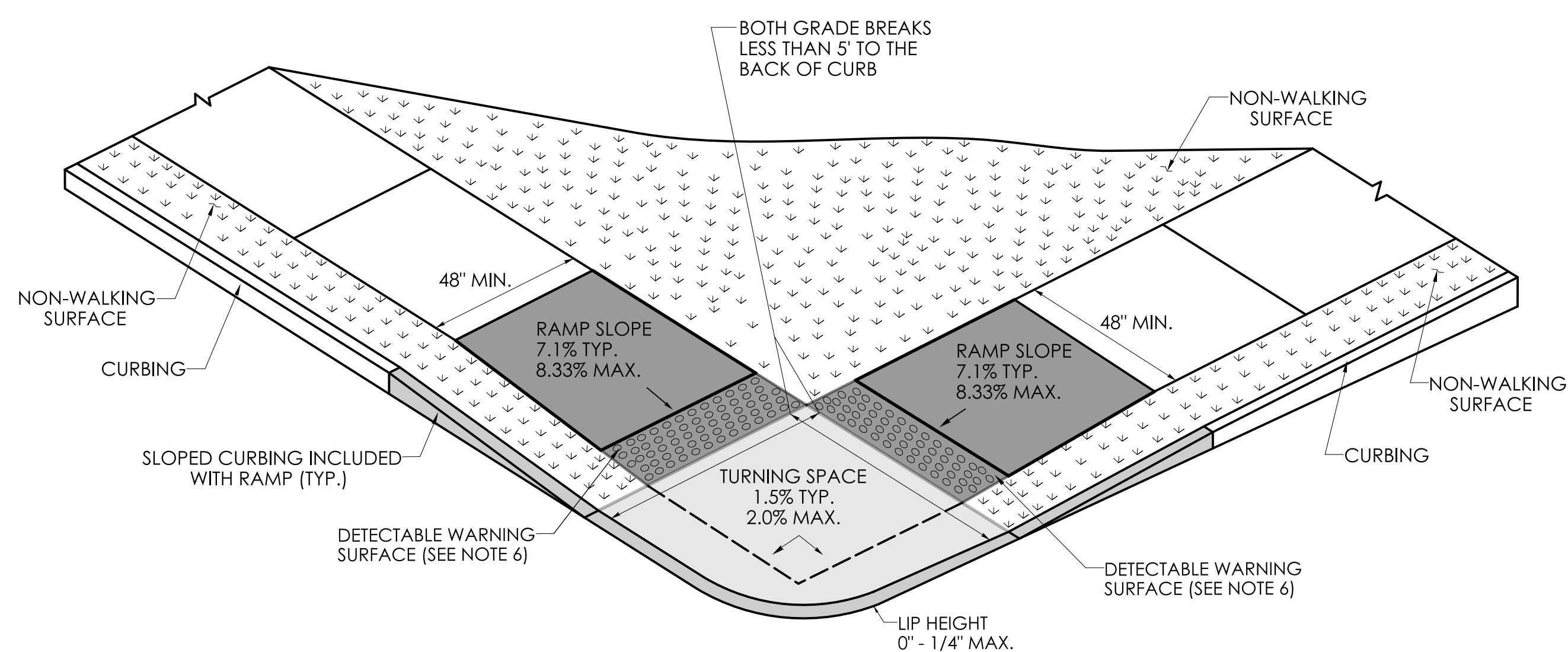
**TWO DIRECTIONAL PERPENDICULAR RAMP WITH A GRADE BREAK TO BACK OF CURB OF 5' OR LESS (TYPE 1)**



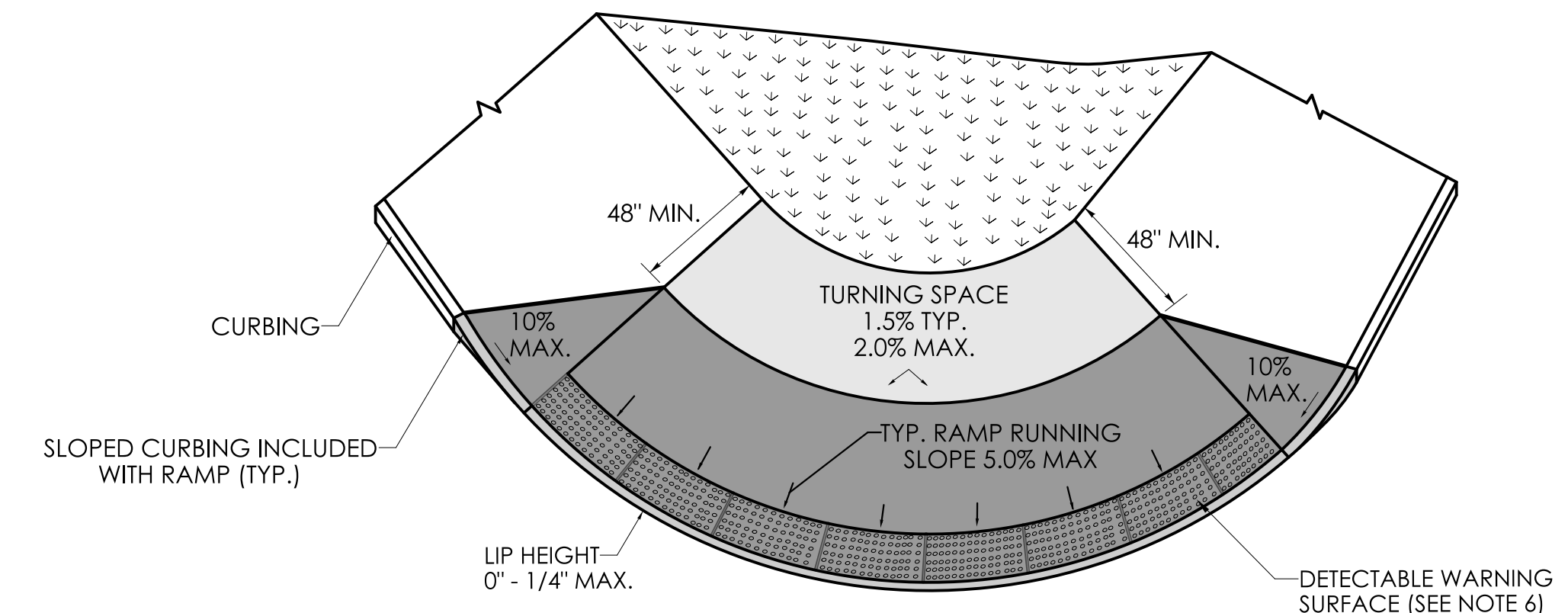
**TWO DIRECTIONAL PERPENDICULAR RAMP WITH GRADE BREAK TO BACK OF CURB GREATER THAN 5' (TYPE 2)**



**TWO DIRECTIONAL PERPENDICULAR RAMP WITH A GRADE BREAK TO BACK OF CURB GREATER THAN 5' WITH NON-WALKING SURFACE (TYPE 4)**



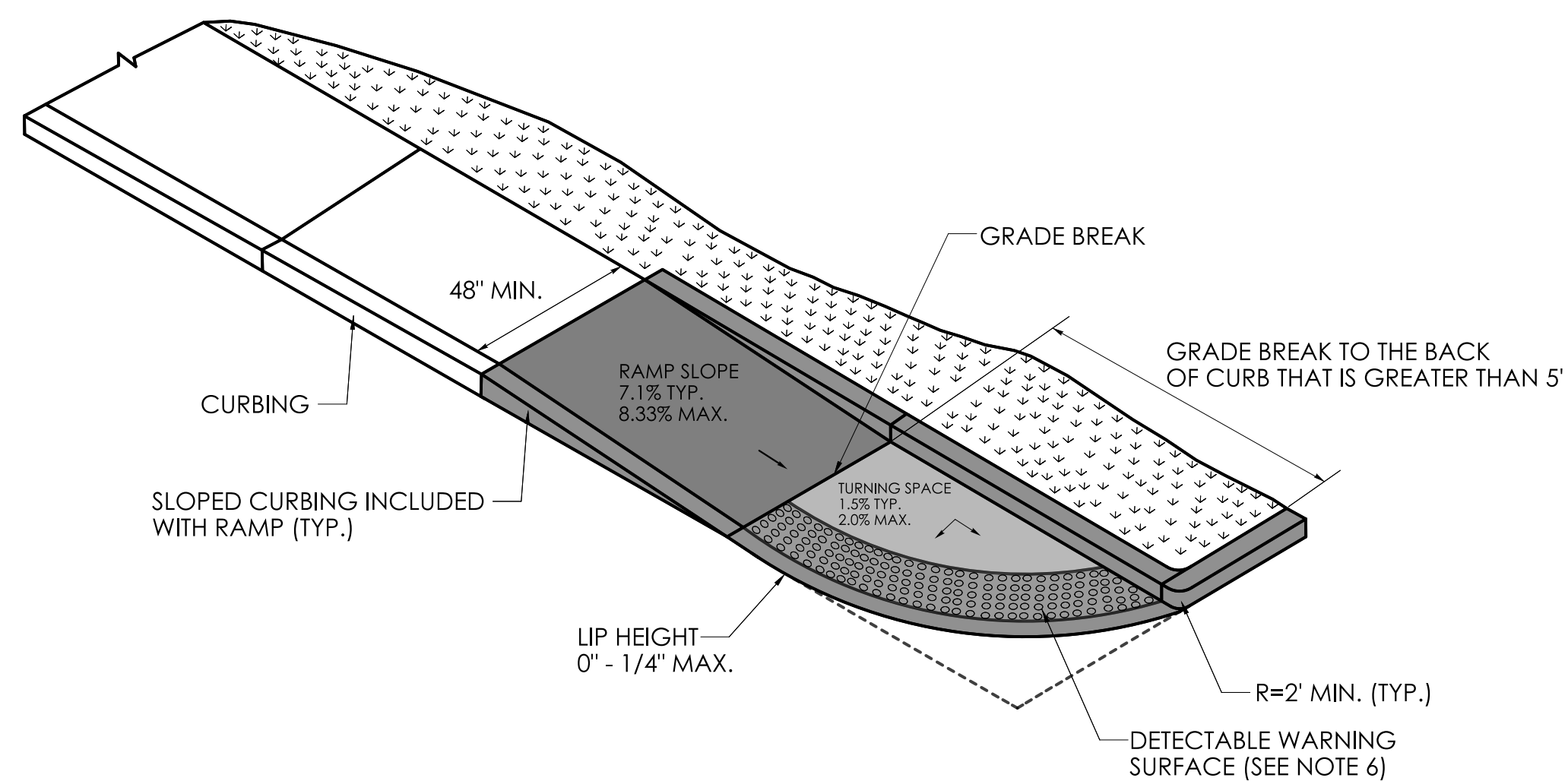
**TWO DIRECTIONAL PERPENDICULAR RAMP WITH A GRADE BREAK TO BACK OF CURB OF 5' OR LESS WITH NON-WALKING SURFACE (TYPE 3)**



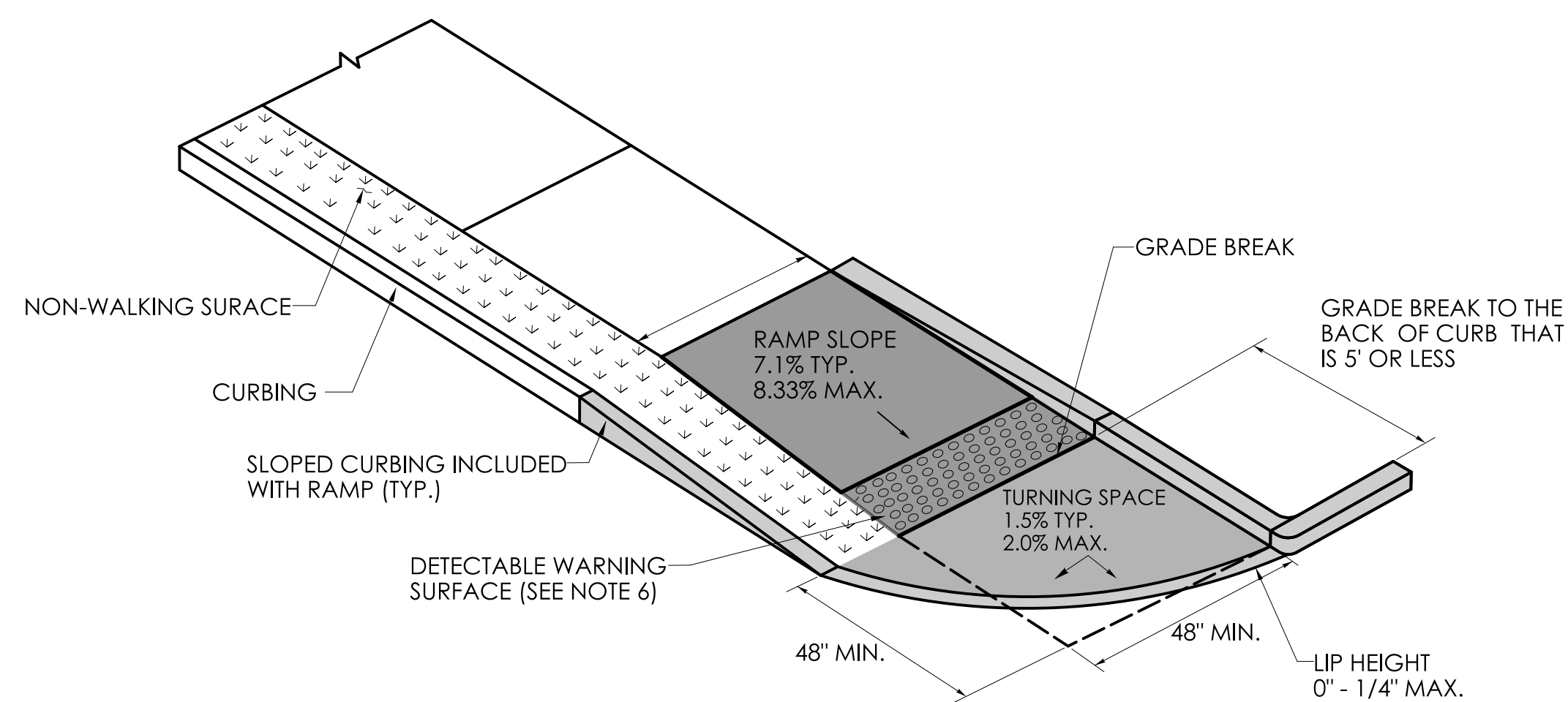
**BLENDED TRANSITION WITH TURNING SPACE AT TOP (TYPE 5)**

- GENERAL NOTES:**
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
  2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED 1/4 INCH.
  3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
  4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
  5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
  6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
  7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
  8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
  9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

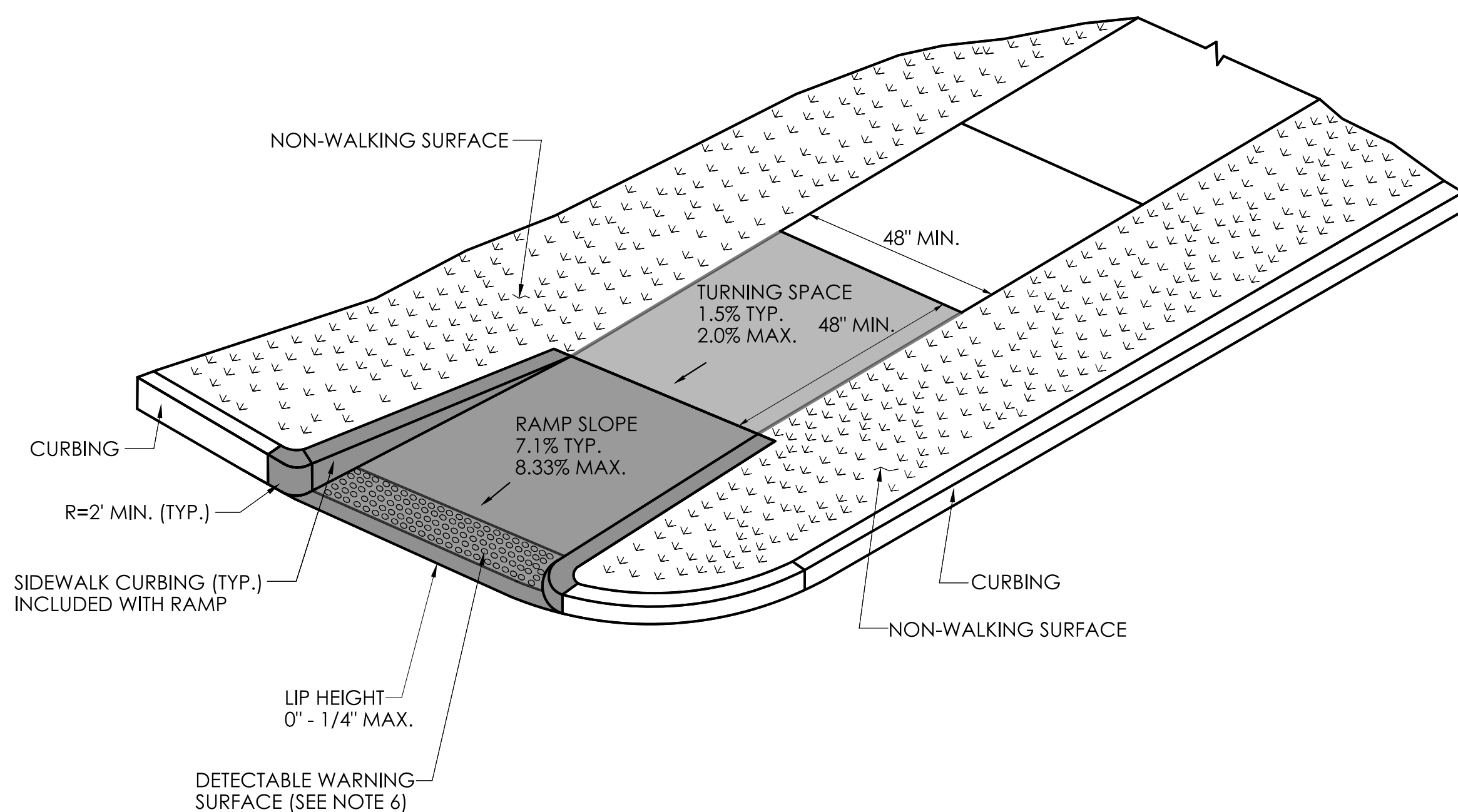
REV.	DATE	REVISION DESCRIPTION



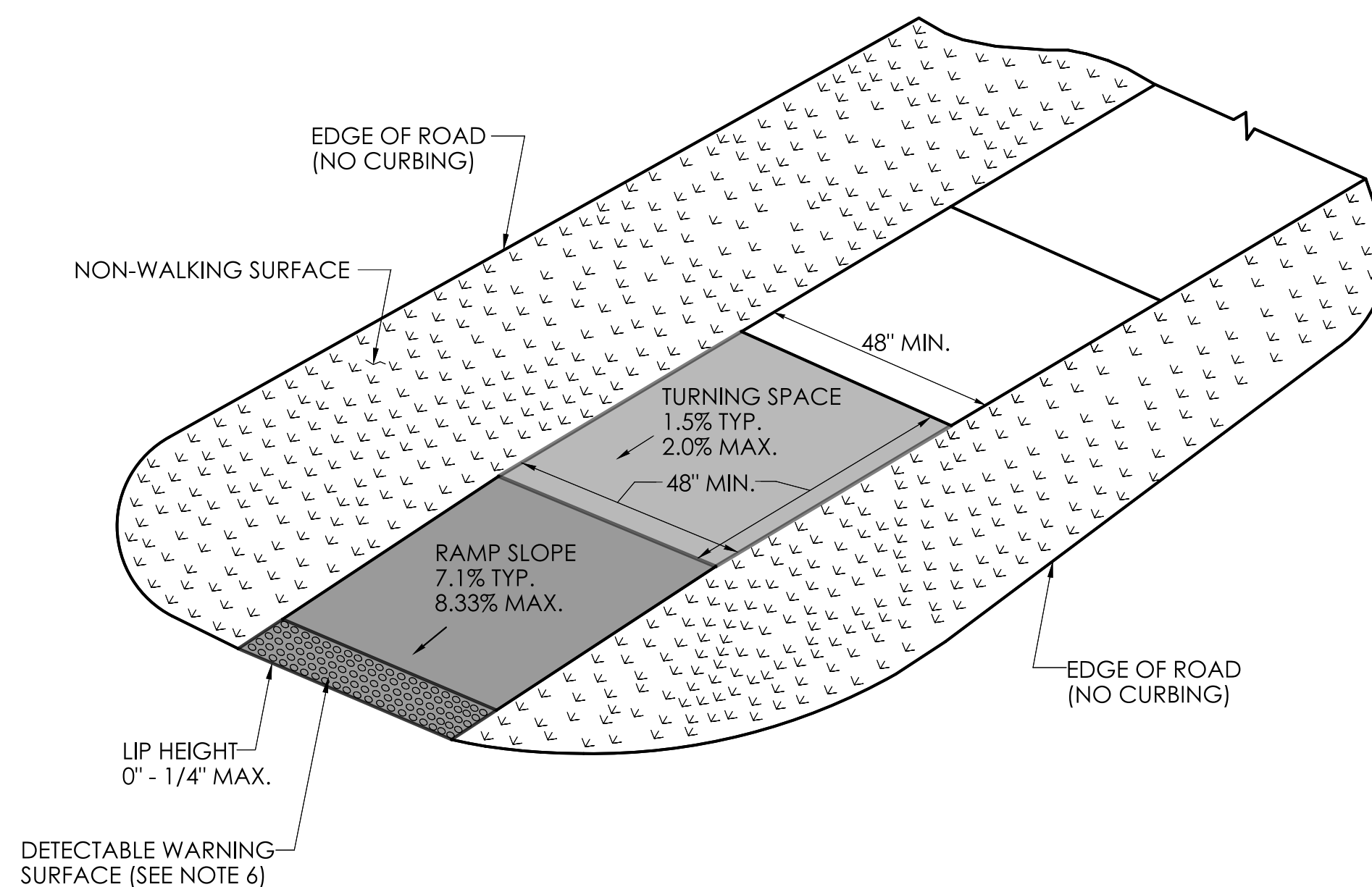
**SINGLE DIRECTION PERPENDICULAR RAMP  
GRADE BREAK TO THE BACK OF CURB  
GREATER THAN 5'  
(TYPE 14)**



**SINGLE DIRECTION PERPENDICULAR RAMP  
WITH GRADE BREAK TO THE BACK  
OF CURB THAT IS 5' OR LESS  
(TYPE 15)**



**SINGLE DIRECTION PERPENDICULAR RAMP  
RETURN CURB WITH NON-WALKING SURFACE  
(TYPE 16)**



**SINGLE DIRECTION PERPENDICULAR RAMP  
NO CURB WITH NON-WALKING SURFACE  
(TYPE 17)**

- GENERAL NOTES:**
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
  2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED 1/4 INCH.
  3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
  4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
  5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
  6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
  7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
  8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-07.
  9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

REV.	DATE	REVISION DESCRIPTION